



# Nicholtown Master Plan



## DESIGN GUIDELINES

May 2004

*Prepared For:*  
City of Greenville and the Greenville Housing Authority

*Prepared By:*  
URBAN COLLAGE, INC. in collaboration with URS, Robert Charles Lesser & Co. and J. Peters & Assoc.





## **PREFACE:**

The following pages represent a clear and concise guide to encouraging new development in Nicholtown that is compatible with the character of the existing community. These guidelines are intended to be straight forward, easy to use and a pre-cursor to a more detailed set of standards associated with a potential new zoning code (timeline TBD).

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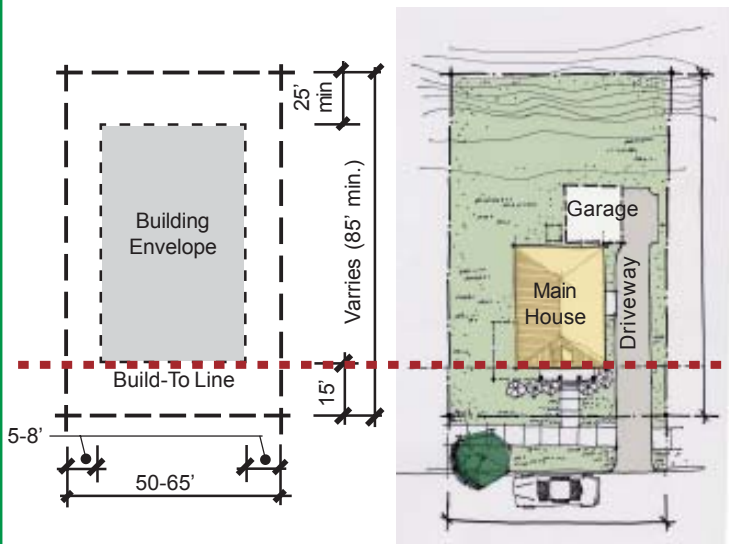
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## Lot Layout - Driveway Lot



### INTENT:

New detached housing that is compatible with the existing fabric of the community and that promotes a walkable environment.

### LOT LAYOUT STANDARDS:

**Lot Width-Driveway Lot:** 50 ft. min. (preferred); 65 ft. max.

**Lot Width-Alley Lot:** 35 ft. min. (preferred); 50 ft. max.

**Lot Depth:** Varies, 85 ft. min.

**Side Setbacks:** 5 ft. min.; 15 ft. max (zero lot line development allowed in locations where at least 8 lots are contiguous in a block)

**Rear Setbacks:** 25 ft. min. (accessory structures shall be 6 ft. min.)

**Front Build-To Line (Infill):** In cases where there is a predominance of existing adjacent houses to remain, infill housing shall be generally aligned.

**Front Build-To Line (Subdivision):** In cases where there is a lack of existing houses to remain, infill housing shall have a 20 ft. Front Build-To Line.

**Driveways/Parking Pads:** Shall at no point be wider than 9 ft. (except in rear yard or connecting to an alley) and shall only be located to the side and/or rear of a house (no front yard locations). Driveways may be located outside of the Building Envelope but shall be at least 2 ft. away from the side property line.

**Garages/Accessory Structures:** Shall be recessed at least 20 ft. from the front house facade. Only one garage car opening shall be allowed parallel with the street (unless obscured from public view); 2-car garage openings are allowed but shall be perpendicular to the street (or obscured from public view)

**Sidewalks:** All houses shall have a 4 ft. min. sidewalk connecting the front porch to the public sidewalk

### ARCHITECTURAL STANDARDS:

**Building Height:** 2.5 stories max. (from the street); no point of the structure extending above 35 ft. (tall floor-to-floor heights are encouraged).

**Building Width:** 18 ft. min.; 50 ft. max.

**Roof Massing:** All houses shall have at least one gable or dormer facing the public street and all roofs shall have a 12" min. overhang

**Roof Pitch:** Roof pitches shall be between 6/12 and 16/12 (porch roofs & non visible areas may be less)

**Front Porches:** All new houses shall have front porches that are at least 12 ft. wide and 6 ft. deep (wrap-around porches encouraged on corner lots).

**Porch Columns:** All front porches shall contain at least 2 porch columns; columns widths/proportions shall increase as column spacing increases. Porch columns shall include base and capital trim.

**Front Doors/Stairs:** Front doors and front porch stairs shall face the public street.

**Finished Floor Height:** 30 in. above grade min. (exceptions considered for accessibility reqs.)

**Trim Details:** 4" wide min. (or 1 brick course) trim shall be used at windows, doors, corners, cornices, eaves, rakes and fascias.

**Acceptable Siding Materials:** Wood, synthetic wood board, stone, brick, stucco (horizontal patterns only).

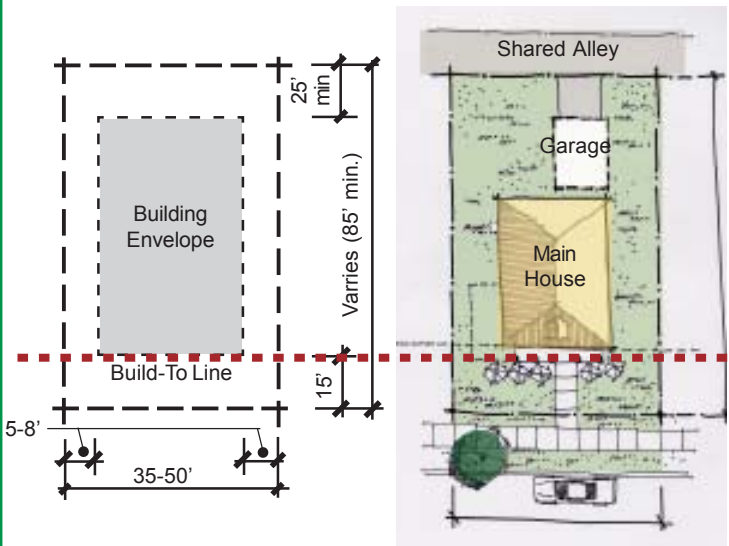
**Discouraged Siding Materials:** Vinyl, block, synthetic stucco, metal, plastic, plywood.

**Acceptable Roofing Materials:** Asphalt shingles only (no metal, rolled or built-up roofs)

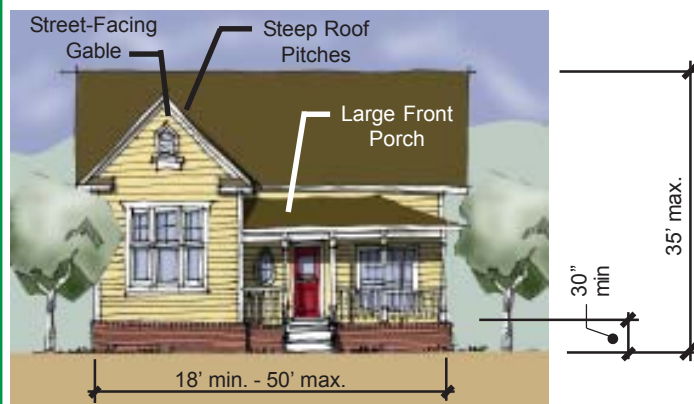
**Chimneys:** Shall be faced in stone, brick or stucco.

**Fenestration:** 30% min. of the front facade shall be fenestrated.

## Lot Layout - Alley Lot



## Architectural Features



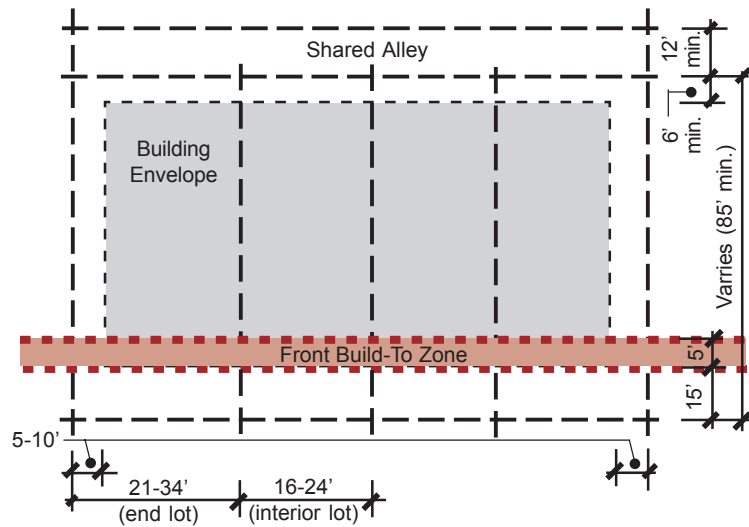
# 1 SINGLE-FAMILY DETACHED GUIDELINES

Prepared By: URBAN COLLAGE, INC.

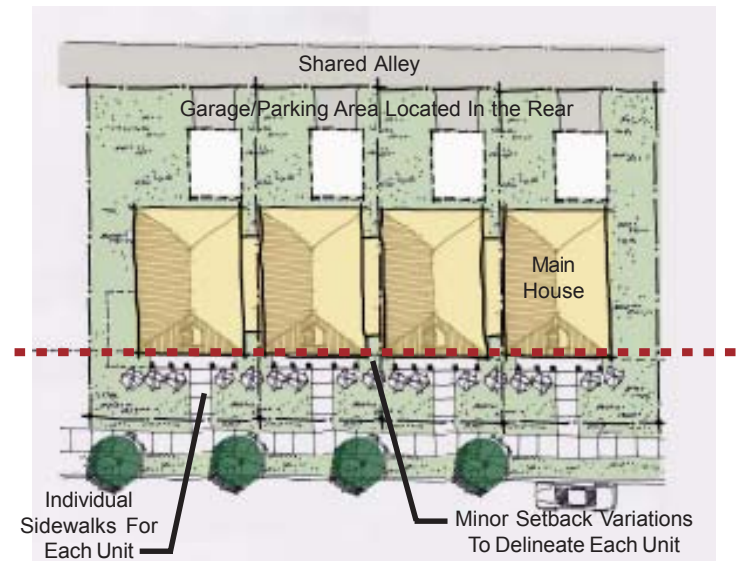
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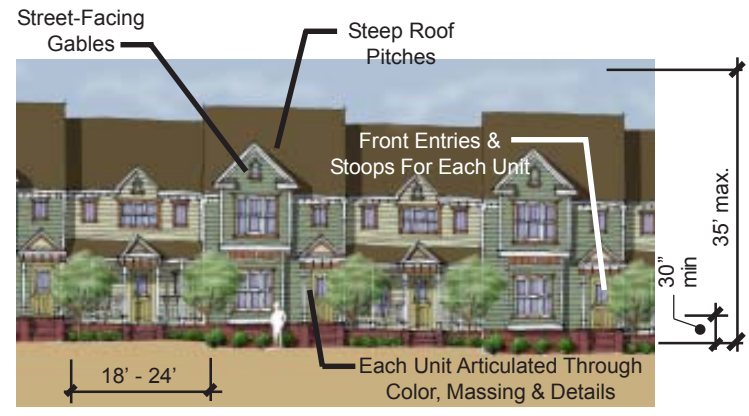
## Lot Layout



## Layout Features



## Architectural Features



### INTENT:

New single-family attached housing that is compatible with the existing fabric of the community and that promotes a walkable environment.

### LOT LAYOUT STANDARDS:

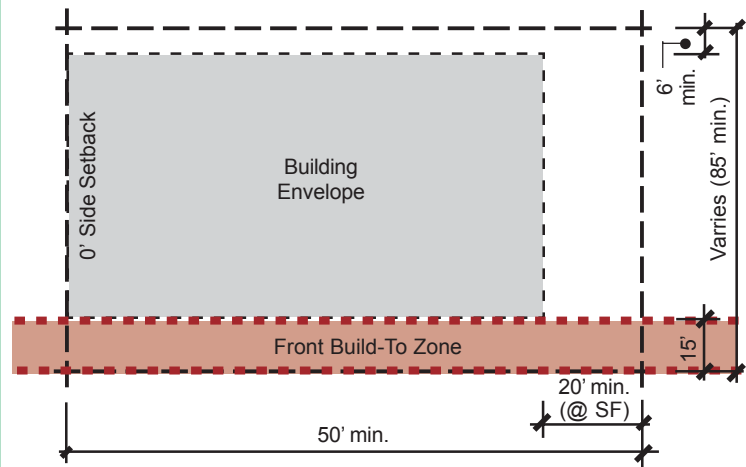
- Lot Width-Interior Lot:** 16 ft. min.; 24 ft. max.
- Lot Width-End Lot:** 21 ft. min.; 34 ft. max.
- Lot Depth:** Varies, 85 ft. min.
- Side Setbacks-End Lot:** 5 ft. min.; 15 ft. max (zero side setbacks required on interior sides of all lots)
- Rear Setbacks:** 6 ft. min. (incl. accessory structures)
- Front Build-To Zone:** Front facades of each unit shall fall within a 5' zone 15' back (thus allowing minor variations to help delineate individual units).
- Minimum Unit Counts:** For each contiguous development property, new attached housing shall contain at least 8 units.
- Driveways/Parking Pads:** All driveways/parking areas shall only be located to the side and/or rear of a unit (no front yard locations).
- Shared Alleys:** All attached developments shall be required to share rear parking access through the use of a shared alley (min. 12' wide). Alleys may be one-way or two-way. To the extent possible, alleys shall be accessed via the side of the block. Where not possible (i.e., infill development in block interiors), alleys shall be accessed via the primary street but shall have no more than 2 curb cuts.
- Garages/Accessory Structures:** Shall be provided only along the rear of the main dwelling structure and shall only be accessed via a shared alley (no front-facing garages).
- Sidewalks:** Each individual unit shall have a 4 ft. min. sidewalk connecting the front porch to the public sidewalk (no shared sidewalks).

### ARCHITECTURAL STANDARDS:

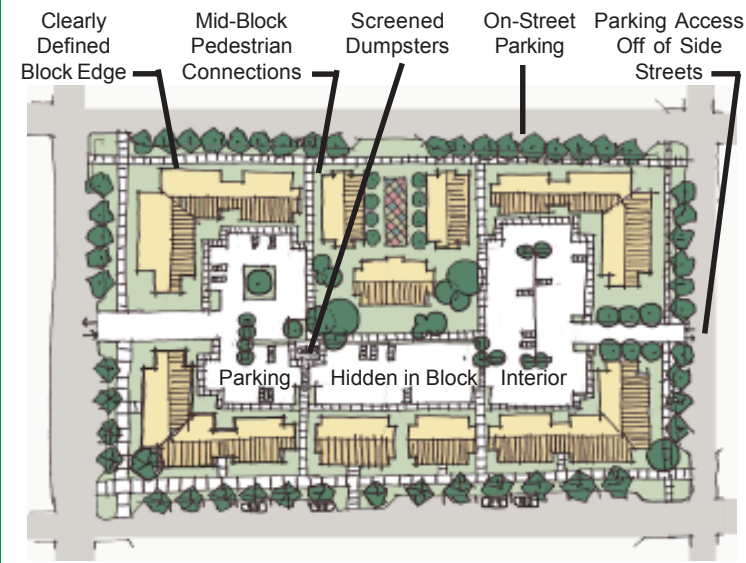
- Building Height:** 2.5 stories max. (from the street); no point of the structure extending above 35 ft. (tall floor-to-floor heights are encouraged).
- Individual Unit Width:** 16 ft. min.; 24 ft. max.
- Roof Massing:** All dwelling units shall have at least one gable or dormer facing the public street and all roofs shall have a 12" min. overhang. Roof massing should be used to delineate each unit from one another.
- Roof Pitch:** Roof pitches shall be between 6/12 and 16/12 (porch roofs and non-visible areas may be less)
- Front Porches:** Each dwelling unit shall have front porches or stoops that are at least 6 ft. wide and 6 ft. deep (wrap-around porches encouraged on corner lots).
- Front Doors/Stairs:** Each dwelling unit shall have a front door that faces the public street.
- Finished Floor Height:** 30 in. above grade min. (exceptions considered for accessibility reqs.)
- Acceptable Siding Materials:** Wood, synthetic wood board, stone, brick, stucco (horizontal patterns only).
- Discouraged Siding Materials:** Vinyl, block, synthetic stucco, metal, plastic, plywood.
- Acceptable Roofing Materials:** Asphalt shingles only (no metal, rolled or built-up roofs)
- Chimneys:** Shall be faced in stone, brick or stucco.
- Fenestration:** 30% min. of the front facade shall be fenestrated.



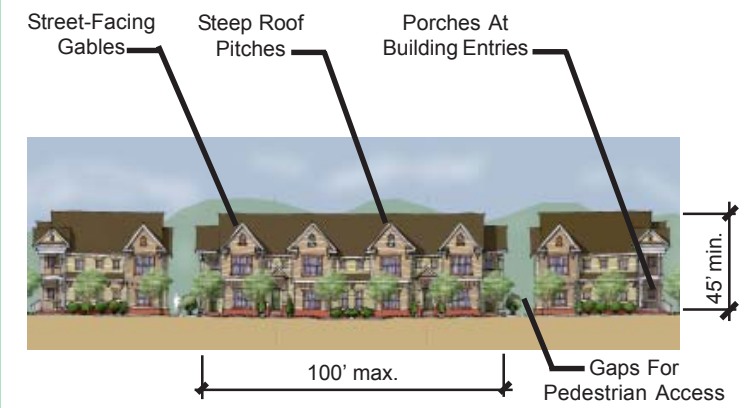
## Lot Layout



## Layout Features



## Architectural Features



### INTENT:

New multifamily housing that is compatible with the existing fabric of the community and that promotes a walkable environment.

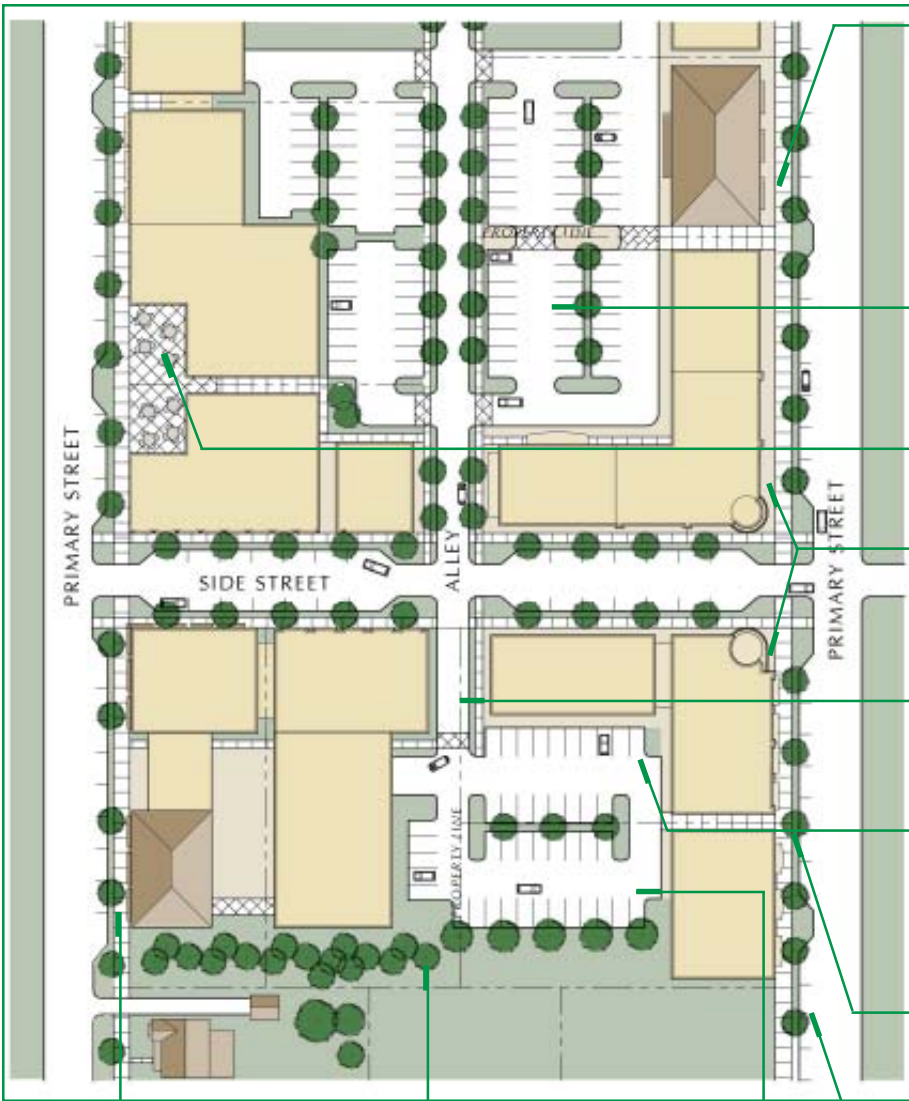
### LOT LAYOUT STANDARDS:

- Lot Width:** 50 ft. min.; no max.
- Lot Depth:** Varies, 85 ft. min.
- Side Setbacks:** 0 ft. min. (20' ft. min adjacent to single-family attached or detached housing)
- Rear Setbacks:** 6 ft. min. (incl. accessory structures)
- Front Build-To Zone:** Front facades shall fall within the first 15' (variations within this zone along the facade and among buildings is encouraged).
- Minimum Unit Counts:** For each contiguous development property, new multifamily housing shall contain at least 8 units.
- Parking Counts:** 2 spaces per dwelling unit max. in off-street locations.
- Off-Street Parking Areas:** Parking areas shall be locating in the rear yard only (no front yard locations).
- On-Street Parking Areas:** The provision of on-street parking is stongly encouraged and should be used to reduce the overall demand for off-street parking.
- Driveways/Shared Alleys:** All new multifamily developments shall be required to share parking access through the use of a shared alley or drive (min. 12' wide). Alleys/drives may be one-way or two-way. To the extent possible, alleys shall be accessed via the side of the block. Where not possible (i.e., infill development in block interiors), alleys shall be accessed via the primary street but shall have no more than 2 curb cuts.
- Garages/Accessory Structures:** Shall be provided only along the rear of the main buildings and shall only be accessed via a shared alley or drive (no front-facing garages).
- Block-Edge:** buildings should be laid out so as to clearly define the block edge.
- Sidewalks:** Each Individual building shall have a 6 ft. min. sidewalk connecting the entry way to the public sidewalk.
- Utility/Refuse Areas:** Shall be screened from public view.
- Perimeter Fencing:** Is not allowed except between and connecting to structures (if gated).
- Pedestrian Layout:** Structures shall be laid out in such a manner that it is easy for pedestrians to reach the public sidewalk through frequent gaps between structures.

### ARCHITECTURAL STANDARDS:

- Building Height:** 3 stories max. (from the street); no point of the structure extending above 45 ft. (tall floor-to-floor heights are encouraged).
- Building Massing:** Structures shall be designed so as to not overwhelm nearby single-family housing (i.e., frequent/smaller structures rather than fewer/longer structures); no structure shall be longer than 100'
- Roof Massing:** All structures shall at least 2 gables or dormers facing the public street and all roofs shall have a 12" min. overhang
- Roof Pitch:** Roof pitches shall be between 6/12 and 16/12 (porch roofs and non-visible areas may be less)
- Front Porches/Stoops:** Shall be incorporated into every structure and be at least 6 ft. wide and 6 ft. deep (wrap-around porches enouraged on corner locations).
- Finished Floor Height:** 30 in. above grade min. (exceptions considered for accessibility reqs.)
- Acceptable Siding Materials:** Wood, snythetic wood board, stone, brick, stucco (horizontal patterns only).
- Discouraged Siding Materials:** Vinyl, block, sythetic stucco, metal, plastic, plywood.
- Acceptable Roofing Materials:** Asphalt shingles only (no metal, rolled or built-up roofs)
- Chimneys:** Shall be faced in stone, brick or stucco.
- Fenestration:** 30% min. of the front facade shall be fenestrated.





Front setbacks of new commercial buildings should be aligned with existing buildings to create a clearly defined edge. In the absence of an existing setback line, front setbacks along non-arterial roads should be no more than 25 ft. away from the curb (i.e. build-to line).

Shared parking for different tenants and uses is strongly encouraged to minimize the overall supply of parking.

Setback variations to accommodate outdoor cafes / dining are allowed but should be kept to a minimum.

Distinctive architectural treatments / massing on corner lot buildings (on major retail or gateway nodes) is strongly encouraged.

Driveways to adjoining businesses should be combined whenever possible to minimize curb cuts on public streets.

Parking lots, decks and building service areas should be located away from the view of primary streets and accessed via alleys or side streets, whenever possible.

All surface parking lots and decks should be clearly marked and accessible from primary streets.

On-street parallel parking is encouraged whenever possible and appropriate.

A zero setback is promoted in the retail nodes such as Main Street where high pedestrian activity is expected or encouraged.

Single-family areas should be screened from non-residential areas with an 8' wide (min.) landscaped buffer.

All parking lots/decks shall be located within block interiors to minimize their visual impact and should be well landscaped.



Parallel parking on urban "main streets"



Landscaped Parking Lots

# 4 NON-RESIDENTIAL LAYOUT GUIDELINES

Prepared By: URBAN COLLAGE, INC.

Prepared For: City of Greenville and the Greenville Housing Authority





All new residential buildings should be of historically compatible design in terms of architectural style, details and materials.



Roofs of new infill housing units should be of simple form and consistent with existing historic housing. Roofs should have a pitch of at least 8 inches for every 12 inches and an overhang of at least 12" wide.



Front Doors should be visible from the street.



Windows should be of vertical proportions (double-hung windows). Shutters should be sized to match the proportions of the windows.



The use of decorative features such as bay windows and brackets is encouraged.



Front porches should be included as a design feature as frequently as possible.



The use of architectural details is strongly encouraged.



Multi-family housing should be of an appropriate scale and historic character in keeping with the rest of the neighborhood.



NICHOLTOWN DESIGN GUIDELINES

5 GENERAL RESIDENTIAL GUIDELINES

Prepared By: URBAN COLLAGE, INC.  
Prepared For: City of Greenville and the Greenville Housing Authority





The architectural design, details and materials of new non-residential buildings should be compatible with existing adjacent structures.



Commercial buildings offering a mix of compatible uses (i.e. office or residential above ground floor retail) are strongly encouraged.



All new non-residential buildings should address the street. Main entries should be oriented toward major streets.



Non-residential buildings should be constructed of brick, stone, or other high-quality finishes. Exposed concrete blocks or metal sided buildings are strongly discouraged.



Building facades should be articulated with canopies, porches, cornices, roof lines and window details to avoid monotonous blank facades.



Canopies and awnings may extend over the public sidewalk provided obstructions are kept to a minimum clear height of 8'.



Business signs incorporated on building facades are recommended (rather than monument signs). Wall signs perpendicular to building face that are more visible for pedestrians are strongly recommended.



Commercial buildings should include large storefront windows and awnings or canopies to encourage active pedestrian use.



Active public uses such as retail shops and outdoor cafes are encouraged on the ground floor of buildings.



# NICHOLTOWN DESIGN GUIDELINES

## 6 GENERAL NON-RESIDENTIAL GUIDELINES

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The streetscape should frame and offer a variety of experiences. In general, denser developments should be provided with wider sidewalks and more pedestrian amenities (such as benches, landscaping, etc.)



The nature and intensity of streetscape improvements may vary by street. However a consistently designed set of materials should be utilized (street lights, benches, bike racks, trash receptacles, etc.)



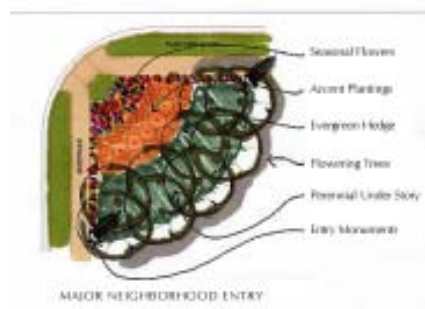
For residential streets, sidewalks (6' min.) should be separated from the roadway with a planting strip (3' min.) which shall include street trees and street lights (where feasible).



For storefront streets sidewalks should be provided with a clear zone (10' min.) adjacent to the building face and a furniture zone (5' min.) along the edge of the sidewalk where possible.



In more urban commercial areas, an additional transitional area (15' max) can be provided for outdoor dining/terraces or public gathering.



Street intersections identified as major entry points into the neighborhood should be properly landscaped and signed to establish a visual gateway for motorists and pedestrians.



All new public spaces should be accessible from the street and/or the pedestrian greenway and linked via a network of pedestrian trails and walkways.



Public spaces should be well-lit, secure and provided with proper directional signage. Landscape design should consist of local flora.



Appropriate pedestrian amenities should be provided in public parks and open spaces. Amenities include gazebos, benches, grills, drinking fountains, play equipment, etc.



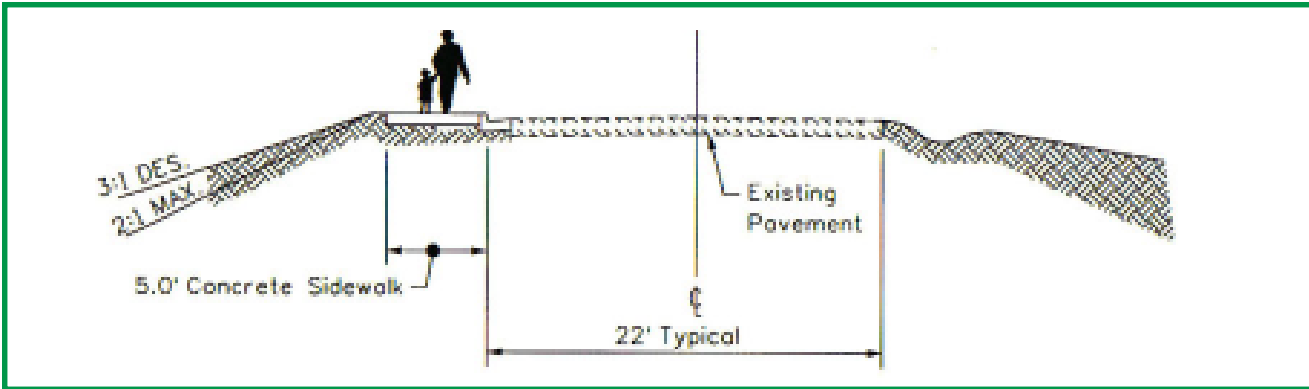
NICHOLTOWN DESIGN GUIDELINES

7 PUBLIC SPACE GUIDELINES

Prepared By: URBAN COLLAGE, INC.

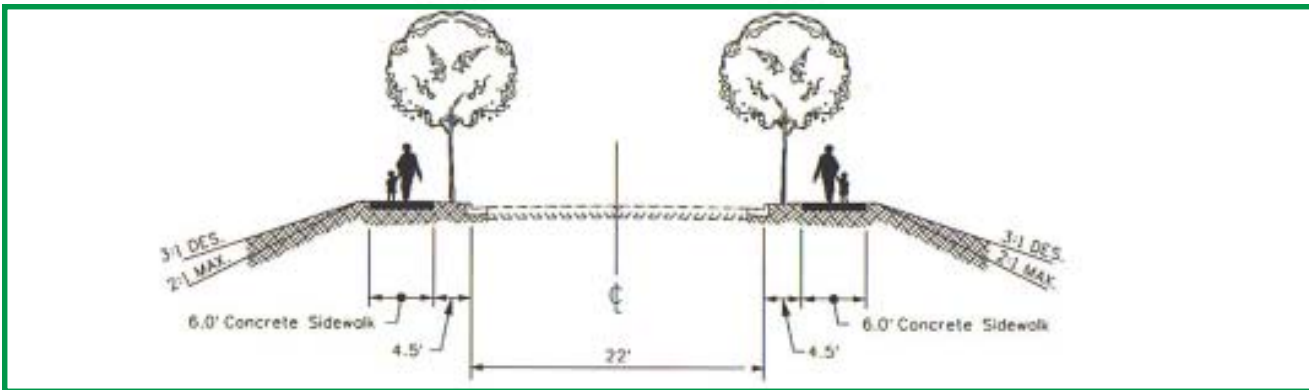
Prepared For: City of Greenville and the Greenville Housing Authority





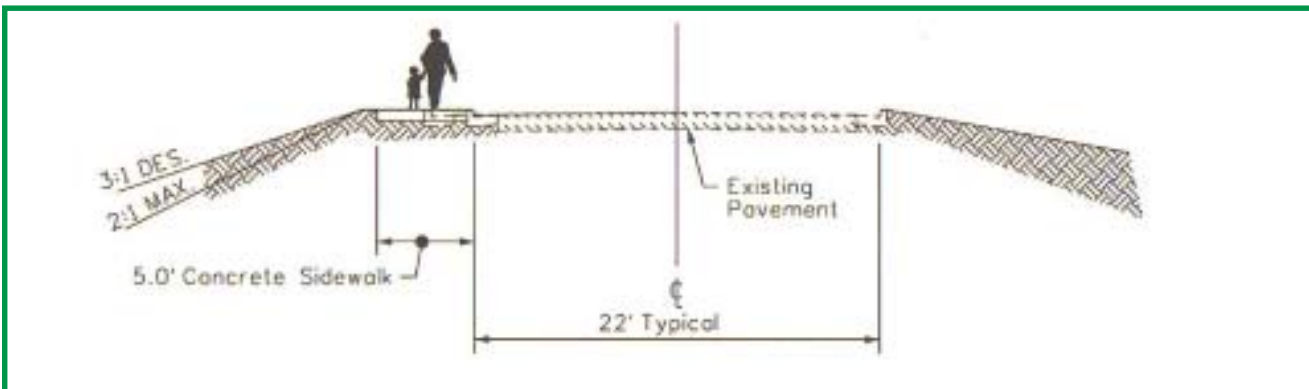
**Typical street section for Alameda Street, Glenn Road and Greenacre Road.**

The typical road section should include a 5'-00" minimum continuous sidewalk on one side of the street and a 22'- 00" minimum roadway width.



**Typical street section for the Allendale Lane Extension, the Webster Street Extension, the Beck Street Extension, Clark Street, the McCullough Street Extension and Roads A & B.**

The typical road section should include a 6'-00" minimum continuous concrete sidewalk on both sides of the road with a 4'-00" planting area and a 22'-00" street right of way.



**Typical street section for Rebecca Street.**

The typical road section should include a 5'-00" minimum continuous sidewalk on one side of the street and a 22'- 00" minimum roadway width.

